

TREAD AND TRACK



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AUSTRALIAN WAR MEMORIAL

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South Vietnam, September 1971. Gunner Brian Chipman of Youngtown, Launceston, Tasmania, a driver with 12th Field Regiment, Royal Australian Artillery (RAA), using a hose to clean one of the unit's trucks in preparation for its return to Australia.

Photo credit: Australian War Memorial

Editorial

Our first Club newsletter for 2024.

If you have any projects currently underway it would be great if you could provide an article for the Newsletter on your restoration project. I've done a quick write-up on the challenges I'm currently facing with the refurbishment of the suspension for Bren Gun Carrier 2814 for this newsletter to give an idea of what to provide. I know there are a few projects out there so I look forward to receiving some articles from Club Members.

Also, if you have any suggestions for Clubs events or trips please let me know so it can be included in the Club Events Calendar.

Ian Patrick – Editor

Defence Transport Heritage Tasmania

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From the President

Hi all,

Hope everyone had a great Christmas break and happy new year, here we are in 2024!
Due to work commitments, we didn't get a chance to get out and about as much as we hoped,
The MUTT did however get a trip to Latrobe over the new year which was enjoyed by all! (hearing protection issued of course)

We discussed a trip possibly to the West Coast at the Christmas Dinner, might see if we can organise a trip in the next few months.

Beginning of March we have the AGM and BBQ.

Want to thank Alan and Lyn for letting us use their house again. Much appreciated to you both.

A week later a few of us are heading to Corowa for a week, and a fast approaching Anzac Day the following month, so a busy couple of months for the club!

The "Big One" we need to discuss at the AGM is "Incorporation" of the club.

Ian and Ann will be attaching more info about this in the news letter and emails.

And I'm sure there will be plenty of other topics of discussion.

Hope you can all make it!

Thanks

Brad Schramm

President DTHT

Upcoming Club Trips or Events Schedule

Below is a list of prospective upcoming events for Club members.

Details will be issued via email following confirmation if required.

Event	Location	Date and Time
2024		
Club Lunch	George Town plus visit to George Town RSL Museum	TBA
Annual General Meeting	Walker Residence, 46 Laycock Rd Spreyton	Saturday 2 March 2024 – Lunch at 11:30am – Meeting at 1:30pm
Corowa Swim-in (Year of the Blitz)	Corowa, NSW	11 March to 17 March 2024
HAMST Military History Fair	HAMST Club Rooms Glenorchy	11 and 12 October 2024
Tasmanian Heritage and Militaria Fair	AgFest Site 415 Oaks Rd, Carrick TAS 7291	29 November 2024 to 1 December 2024
Christmas Lunch	TBA	TBA
2025		
Annual General Meeting	TBA	TBA
Corowa Swim-in (Year of the 6x6)	TBA	TBA

Please advise via email if you have any Club events ideas which you would like added to the schedule for the next Newsletter issue.



Information for Members

Incorporation of the DTHT

See below a brief summary prepared by Richard Green on the possible Incorporation of the DTHT as discussed at the previous AGM.

We would like to put the proposal to incorporate DTHT to a vote at the 2 March 2024 Annual General Meeting so please make yourself familiar with the proposal. Further details will be provided to members in advance of the AGM.

Why Incorporate?

- Incorporation gives a group or association a defined legal status as opposed to just individual persons within a group. For example, if an organisation wants to enter into an agreement, lease, grant application or funding agreement, they need to be a legally defined organisation.
- We already operate under a constitution, however the model rules give a more formal layout to this information, and it can be adjusted in any way we please, the model rules are simply a basic layout suggestion.
- Any agreements could be made as, for example, the DTHT Inc, as opposed to Bob Jones and Bill Murray as individuals.
- One example on the website is if we had leased a premises as a clubroom for example, and James has a fall, only the DTHT Inc, not individual members can be sued in that instance. This isn't exactly an issue for our scenario.
- If we were looking at grants, for the most part grants will only be considered from Incorporated associations or businesses.
- Currently we have lost our dtht.org.au website name because we are no longer eligible because we are not incorporated, this is one of the reasons I personally think we should move in this direction.

Why to not incorporate?

This list is fairly short:

- You don't have to pay the incorporation annual return fee of just under \$50.
- We would not have to comply with the requirements of the incorporations act and constitution, which we essentially do already.

Information for Members

VC Registrations

Vehicle inspections are now able to be undertaken for people wishing to register a vehicle on Club (VC) Registration. Please contact a Club Office or Committee member as listed on Page 2 to arrange an inspection with a Club Inspector if you have a vehicle ready to inspect.

At this stage it is intended that the Club only allows VC Registration of Club themed vehicles (i.e. Military Vehicles).

Please also read the requirements of VC Registration on the Service Tas website so you are aware of how the VC Registration applies. Link below or search on the Service Tasmania Website for VC Registration.

[Apply to register a vintage vehicle or street rod | Service Tasmania](#)

Some key information extracted from the website is provided below:

Vintage and street rod registration is known as club event registration. You must be a member of a recognised car club to have vehicles registered under club event registration.



- *You will have conditions placed on the use of the vehicle including how many days per year you can drive it.*
- *You must have the vehicle inspected by an authorised official of a recognised car club. Contact your car club for details.*
- *If your application is successful, you pay a reduced MAIB premium and you do not pay motor tax.*
- *The vehicle must have been manufactured 30 years ago or more to be classed as a vintage vehicle.*
- *You can use the vehicle for a maximum of 30 days per year outside of club events*.*
- *You must carry a logbook at all times when you use the vehicle outside of club events.*

*Use of a VC registered vehicle can be considered a “club event” if the event is listed in the “Upcoming Club Trips or Events Schedule” in this newsletter. This is in addition to the 30 day allocation of use per year outside of club events.

Information for Members

Club Clothing

Our Club polo shirts and jackets have arrived!

For members wishing to order a polo shirt or jacket with the Club logo, please see the “Club Items for Sale” section of the newsletter.



Club Trips and Event Reports

The Club Christmas Party was held at the Ross Hotel on 25 November 2023. It was well attended by Club members and the food was good.

No photos of the event but thanks to all who attended.



Restoration Projects

Bren Gun Carrier 2814 – Dismantling and Assembling the Suspension – By Ian Patrick

I've been working on Carrier 2814 for quite a few years now and have been putting off working on the suspension because I had a feeling it would be a painful job. The rest of the carrier is pretty much right to go now so I have turned my attention to fixing the suspension and, as I suspected, it is a very painful job!

Carriers have eight sets of coil springs (2 on each bogie, 4 bogies per carrier) and each are made up of a central guide rod with nuts on each end, two outer and two inner coil springs, two balls/cones and two spacer plates.

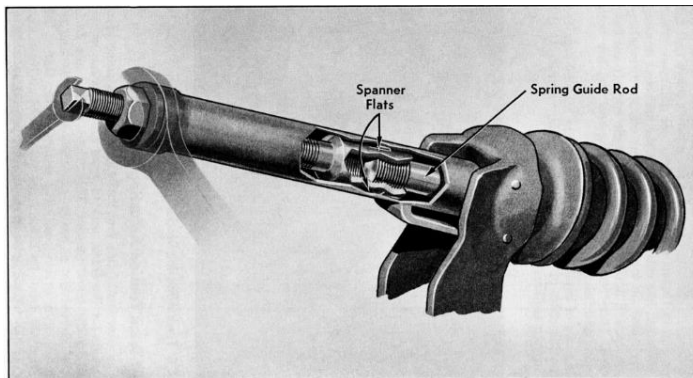


Fig. C.5. Bogie Spring Dismantling Tool

Left: Normally dismantling the suspension of a carrier is a straightforward job with the use of a suspension dismantling tool attached to end of the suspension rod enabling one side of the springs to be compressed and the other side to be removed from the bogie housing as shown in the diagram from the carrier workshop manual.

However, if the carrier has sat around for a long time and moisture has made its way between the rod and ball, it can cause the two to fuse together with rust and prevent the suspension from being dismantled using the technique described above.

I started first on attempting to dismantle the rear LH bogie of Carrier 2814 and was fortunate to be able to remove both sets of coil springs from the bogie as the upper balls were not seized to the rods.

However, the lower balls are seized to the rods and I haven't yet been able to remove them. I have had several goes of heating the balls and rods and trying to press them apart without success. Putting too much heat on the rod and balls can be an issue as it affects the heat treatment of the springs which I

think I may have done on the inner spring of the coil spring set in the right side of the above photo. I have had them soaking in various penetrative fluids also without success. If the balls cannot be freed from the rods, the only option will be to cut the rods with an oxy-acetylene torch to dismantle them. This also risks damaging the springs, particularly the inner spring and will still need to have the rod removed from the ball, probably by drilling it out.

Replacement rods will also be needed with this approach.



Rather than spend too much time on trying to re-use the suspension on Carrier 2814, I decided to dismantle the suspension on my parts carrier (2261) which was mostly unseized but still challenging, with even the removal of the nuts on the ends of the rods sometimes requiring lots of heat and a rattle gun to remove.

Once the spring sets had been removed from the bogies of Carrier 2261, they were tidied up but retained the original patina to match the rest of the carrier then refitted to the bogies on Carrier 2814.



Above: Spring set from Carrier 2261.



Above right: Assembling a spring set on the right-hand side centre bogie of Carrier 2814.



Right: Assembled right hand side centre bogie with both spring sets and return roller fitted.



Left: Left hand side centre and rear bogie on Carrier 2814 with refurbished suspension.

As of early February, all bogies have been refurbished so now onto fitting the track!

Military Vehicles

Deloraine Military Parade YouTube Video – By Ian Patrick

The following screen shots have been extracted from the film of the Deloraine Military Recruitment Parade on Friday 26 September 1941. This was filmed by a member of the Furnage Family and donated to Libraries Tasmania. It is available on Youtube here: [Deloraine Military Parade \(1941\) - YouTube](#) or search “Deloraine Military Parade” on the Libraries Tasmania Youtube channel.

I have left out the analysis of the four Bren Gun Carriers in the parade as I previously covered those in Issue 52 November 2023 of the Defence Transport Heritage Newsletter.

This film is a great resource in identifying the vehicles and equipment being used in Tasmania during this very early period of World War 2. For some perspective on the timeframe of when this film was taken, it was a couple of months before the Japanese attack on Pearl Harbour, so when this was filmed, the US hadn't yet joined WW2.

Some of my vehicle identifications may not be correct as I'm far from being an expert, but these are my best guesses based on what is visible in the footage.



Above: 0min 25 sec – Rear end of a Ford (Marmon-Herrington) LP3 Gun Tractor towing a 4.5inch howitzer with limber followed by another LP3 Gun Tractor also towing a 4.5inch howitzer with limber. The LP3 Gun Tractor is based on the 1939 Ford 91T, identifiable by the “beer barrel” grill. The Gun Tractors used Marmon-Herrington front drive axles and transfer boxes to provide 4x4 capability.

Parked on the opposite side of the street, facing in the opposite direction is what appears to be a Ford utility of the type used by the Army at the time. This may be a military vehicle or may just be a parked civilian vehicle.



Above: 0min 32 Sec – Another LP3 Gun Tractor also towing a 4.5inch howitzer with limber. This is the same type of Gun Tractor as at 0min 25 sec (with the “beer barrel” type grill). The second Gun Tractor in this image has a different grill and is the later 1940 Ford O1T, possibly making it an LP3A Gun Tractor?



Above: 0min 38 sec – A nice view of the rear of the LP3 Gun Tractor pulling a 4.5inch howitzer (?) with limber.



Above: 0min 39 sec – Solo motorcycle of British manufacture, so either a BSA, Aerial, Triumph, Royal Enfield or Norton.



Above: 0min 41sec – Close up of a Ford LP3 Gun Tractor with the “beer barrel” type grill. This one is towing a 2Pdr Anti-tank gun. This shot shows some good features of the LP3 Gun Tractor, with driven Marmon Herrington Front axle and “Roadster” cab.



Above: 0min 42sec – Rear shot of the same truck shown at 0min 41 sec showing the 2pdr Anti-tank gun.



Above: 0min 43sec – Close up shot of the 2pdr Anti-tank gun.



Above: 0min 50 sec – Two more motorcycles, both likely to be of British manufacture. The riders have white arm bands, possibly indicating Military Police?

They are followed by a Ford utility.



Above: 1min 59 sec – Two Indian Pattern Army Ambulance's. Probably 1940 Chevrolet WA 30-cwt.



Above: 2min 02 sec – The same Indian pattern ambulance bottom left in this shot as the top right in the footage at 1min 59sec. Followed by a third Indian Pattern Ambulance. Behind the last Ambulance is a Chevrolet cargo truck



Above: 2 min 08 sec - Chevrolet 30cwt GS truck



Above: 2 min 12 sec - Ford utility followed by a Ford truck, both with "LAD" stenciled on the windscreen, denoting both as "Light Aid Detachment" vehicles. The Ford truck is closely followed by a solo motorcycle then three combination sidecar motorcycles.



Above: 2min 20 sec – close up shot of a Harley Davidson combination sidecar motorcycle. The registration number appears to be an AIF number T224 but that doesn't align with a motorcycle so may be a civilian registration. The rider appears to be wearing a civilian police uniform so this may be a civilian police motorcycle. I recall seeing a very similar motorcycle combination on display at Pearn's Steam World in Westbury when I was there last.



Above: 2min 22 sec – The second Harley Davidson combination sidecar motorcycle, this one definitely being ridden by civilian policeman, so again this is likely to be a civilian police motorcycle.



Above: 2min 25 sec – The last vehicle in the parade is another solo motorcycle of British manufacture.

For Sale or Wanted

- **For Sale:** 1942 GMC CCKW 353-6x6 half restored, originally used by Tasmanian Volunteer Brigade.

All hard to get items available 95% complete with many new old stock parts. Cab and chassis repaired and painted ready for assembly. Comes with two spare chassis, plenty of mechanical parts and a pile of second hand parts. \$12,000.

Contact Brad for details of the current owner and seller.



Club Items for Sale

DTHT Hats and Mugs are available purchase. Please contact Ann if interested. Contact details on page 2.

- Hats \$15.00 each (Available in green or cream colour)
- Mugs \$15.00 each



We now also have Soft Shell Jackets, Vests and Polo Shirts available for members via order (order to size). Please contact Ann if interested. Contact details on page 2.



- Soft shell jackets or vests with the Club Logo. Can also have your name embroidered on it. Around \$78.00 each
- Polo shirt with the Club Logo. Can also have your name embroidered on it. Around \$36.00 each